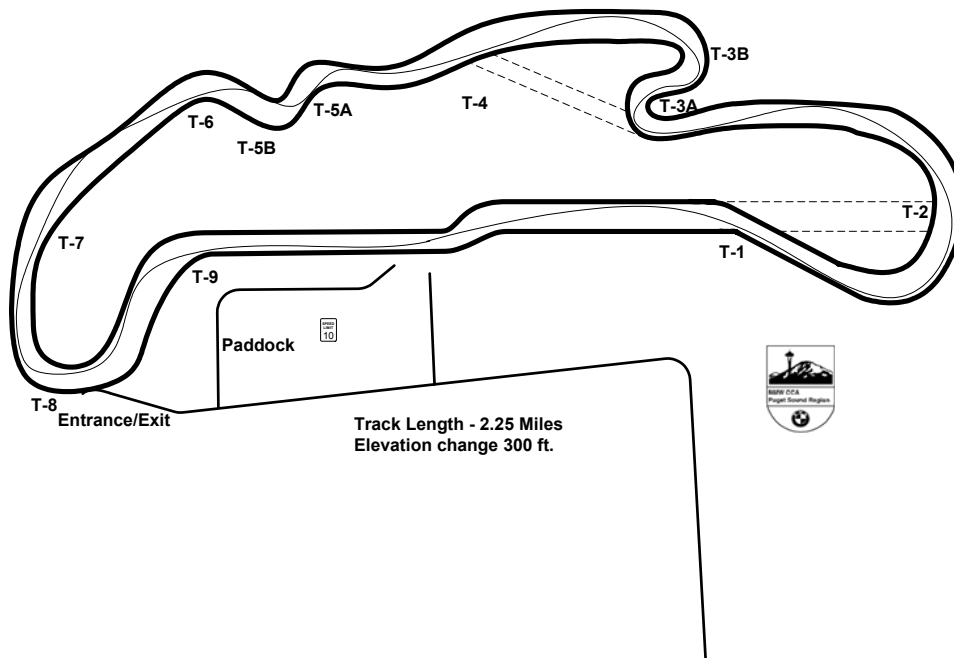


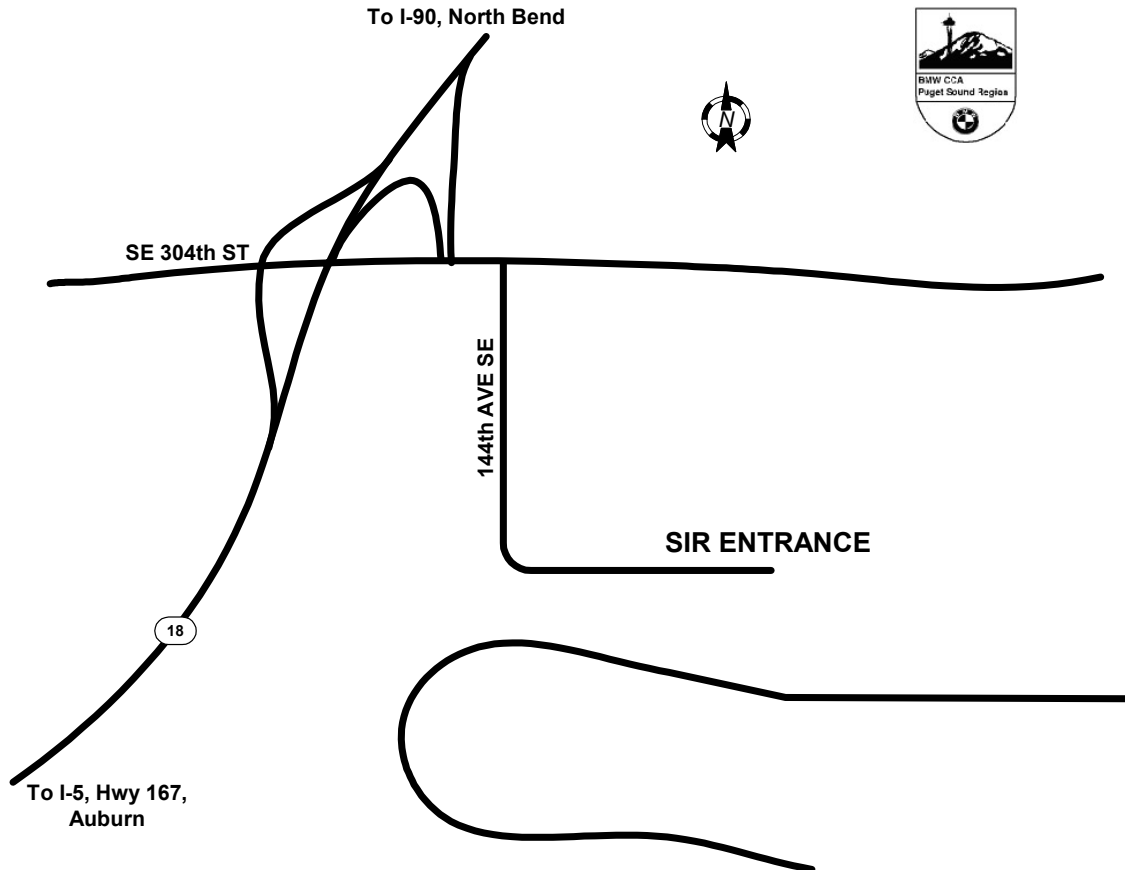
## Pacific Raceways – Kent Washington

**DISCLAIMER:** The following information is provided by the BMW CCA Puget Sound Region as an orientation to this track. It is intended to be used only as a guideline and intended only for use by drivers at BMW CCA events. All drivers are responsible for determining the safest and best approach for themselves and their cars. Under no circumstances will the BMW CCA, its officers, event organizers, instructors or other members be responsible for any consequences to any driver as a result of completely or partially following the recommendations herein. This exclusion applies to events conducted by BMW CCA Puget Sound Region as well as other events where drivers may choose to use these guidelines to assist in learning to drive this track.



This guide is most effective if read once to familiarize yourself with the general track layout. Then come back and review it once you have been on track. It will make much more sense after having seen the track first. The driving “line” is often talked about as if it were a hard and fast rule when in reality it is perceived optimum that is fluid and variable, depending on the performance and handling of each car, the environmental conditions (weather, road conditions, etc.) and in the end, what each driver is trying to accomplish. First and foremost, drive in a safe and controlled manner while being smooth and consistent and never drive over your ability. As the ability of a driver builds, the line may vary and become more aggressive but the goal should be the same – to be smooth and consistent. The driving “line” that the BMW CCA Puget Sound Region teaches can be looked upon as a foundation from which to build an individual’s skill level. It is by no means, the only “line” around Pacific Raceways. Illustrations used in this guide are not to scale but are provided as a learning aid and reasonable effort is made to make them representative of the track.

Pacific Raceways (<http://www.pacificraceways.com>) is a 2.25 mile road course that blends the track facilities into the natural landscape. The track has nine turns plus a kink on the main straight and 125 feet of elevation changes and the natural beauty of the "backside" looks more like a winding country road surrounded by trees than a race track. With two left-hand sweepers, two hairpin corners (one right, then one left) and the combination esses of 5A, 5B and 6, this track offers many challenges while being a very fun track to drive. Pacific Raceways is currently (2003) going through a transformation as new owners are upgrading the facilities and the track itself. Don't be surprised to find the track layout and configuration changing and improving over the next few years.

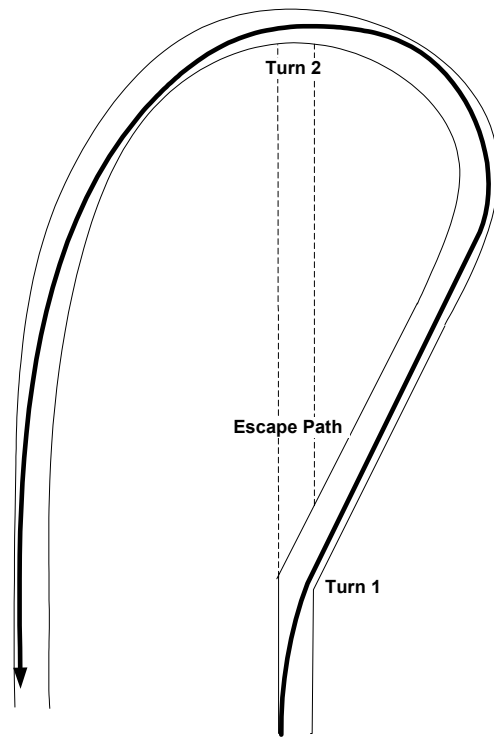


### Glossary of Terms:

Turn-in:	Point at which you begin turning the car toward the apex of a corner
Apex:	Point where you are at your closest approach to the inside of a corner
Track-out:	Point where you have transitioned from the apex to the outside of a corner
Fog line:	Painted line near the edge of the track, on driver's left or right
FIA curb:	Angled curb at the track edge, usually at an apex or track-out point
Even throttle:	Applying the throttle so as to maintain the same speed

**Rain notes:** Generally speaking, Pacific Raceways drains fairly well in rainy conditions. There are a few places on the back side where runoff may flow over the track surface (possibly bringing some dirt with it) at the exit of Turn 3B and near Turn 5A and 5B. Track improvements may solve this problem. The front straight can have standing water in places along the left wall, past the apex of Turn 9, in the area of the transition onto the drag strip and near the turn-in for Turn 1. Wet conditions on the drag strip can be treacherous due to application of traction compounds (VHT) for drag events. Be especially careful in areas of standing water where seams in the asphalt run parallel to your direction.

Turn 1 and Turn 2



**Turn 1:** Turn 1 is the fastest turn on the track (4<sup>th</sup> or 5<sup>th</sup> gear) and is a kink to the right. Turn-in for Turn 1 is from near the left wall. Turn 1 is blind due to the Jersey barrier along the right side. The correct arc will take your apex near the end of the last section of barrier. Just past this barrier is a patch in the asphalt which you should straddle setting up with the right side tires along and parallel to the driver's right fog line. This will provide a straight braking zone for Turn 2. Avoid going wide through Turn 1 as there are uneven areas in the road surface to driver's left. Power should be even or accelerating due to a slight crest in the road surface leading to the chute between Turn 1 and Turn 2. Therefore, if you are uncomfortable with the speed through Turn 1, brake before turn-in then get back on throttle to settle the back end.

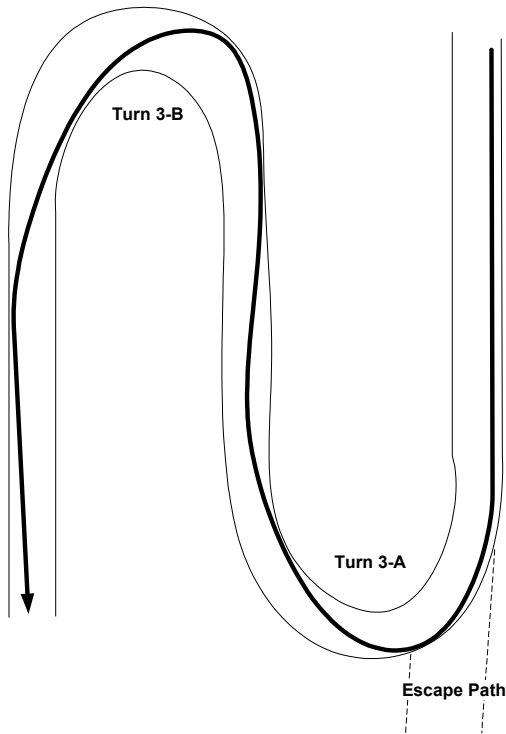


**Turn 2:** Turn 2 is an increasing radius sweeper. The turn station for Turn 2 is on your left, near the center of the turn. Braking for turn 2 will be on driver's right, next to the fog line, where you will down shift into 3rd. Be careful to be either straddling the asphalt seam or have

your left tires to the right of the seam. Having your left tires on the seam will unsettle the car during braking. There is new asphalt on driver's right for entering the track from the hot pits but this tends to not have as much grip as the old surface. Upon *initial* turn-in, straddle the outside asphalt seam and begin to look far ahead to the apex which is as yet out of site. Turn-in for the apex is roughly at the first edge of the front straight run-off road, as the left fog line turns from solid to dashed. As the turn is increasing in radius, turn-in for the apex is very subtle. The apex is just past where a "trail" comes to the edge of the track. After the apex, there is a rough spot in the asphalt surface that you will feel if you are on line. Use all the track and exit on driver's right. Maintain a slight left steering input to bring the car back to driver's left as you crest the hill and head toward Turn 3A.



Turn 3A and Turn 3B



**Turn 3A:** Set up for Turn 3A along the fog line on driver's left. The turn station for Turn 3A is on the hill to your at the end of the braking zone. If you have brake problems due to the drop in elevation combined with the heat generated by braking for Turn 2, there is an escape road straight ahead. As you approach the last breaking cones, downshift to 2<sup>nd</sup> gear and briefly glance to the right to site the apex. Follow the virtual left edge of the turn, resisting the temptation to turn-in early. Turn-in is at the far edge of the escape road as it joins the track surface, forming a "corner" in the asphalt. The apex is late leading to a track-out that is no more than half way across the track. Turn 3A is a "throwaway" corner as not much time can be made here. It is more important to get set up well for Turn 3B.



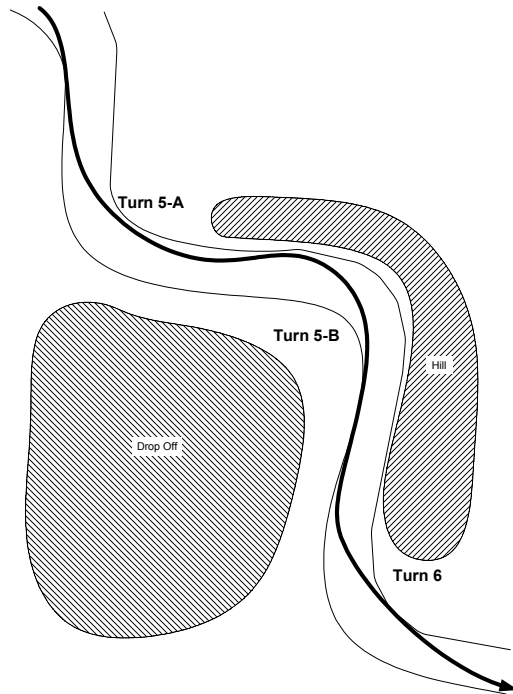
**Turn 3B:** After completing Turn 3A, hustle the car back to driver's right to run parallel to the driver's right fog line before entry to Turn 3B. Depending on your speed exiting Turn 3A, short heavy braking may be required before turn-in. Turn 3B is sharper than Turn 3A and your goal is to slow enough to make a crisp turn-in so that you can get on the power early and make the apex. Turn-in is after the road surface begins to turn left and slightly after the normal turn-in cone. The track surface has a moderate washboard surface which you will feel just after turn-in. Once you know you can make the apex and track-out, get on the power as quickly as possible as this leads onto the back "straight" and is therefore a very important corner.



**Back “Straight” & Turn 4:** The back straight of Pacific Raceways is not a straight at all but follows a gentle curve. Once you exit Turn 3B, line up on driver’s right and sight ahead and to the left. At the crest of the hill is Turn 4. Your goal will be to make this a straight by going first from driver’s right to driver’s left at the crest just past where the Turn 3A escape road rejoins the track. Once you get to the crest you can see ahead to Turn 5A as the road drops away and to the left. Transition roughly to mid track as the road turns up and to the right, approaching turn-in for Turn 5A.



Turn 5A, Turn 5B & Turn 6



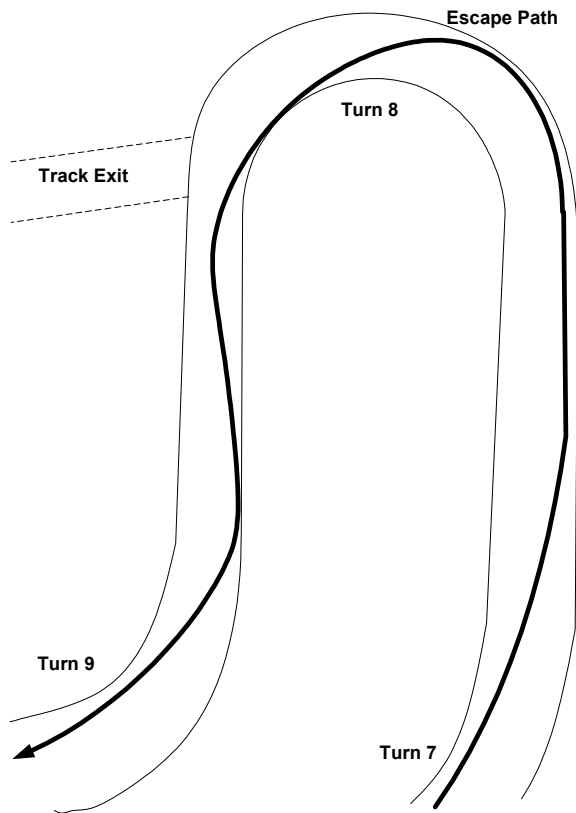
**Turn 5A:** Begin by braking as you come back up the slight hill and complete this, and your downshift to 3<sup>rd</sup> gear, before you turn to the right along the driver's right fog line. The turn station is nearly straight ahead as you approach to warn you of situations beyond this blind corner. You should be even on the throttle through Turn 5A as the road surface is a little rough and there is not a lot of run off to the right or left. As your experience grows, you can push your braking later. Turn-in is from far driver's right and should initially be made at the last cone. The apex is at the corner of the FIA curbing on driver's left. There is a dip in the asphalt here so the even throttle will help settle the back end. Turn in at the last cone and stay to driver's left at the exit along the fog line to set up for Turn 5B (the more important turn). A more advanced line that should not be attempted unless you can proficiently throttle steer is to carry more speed at the entrance, turn-in between the last two cones and track-out to one-third to one-half way across the track. The later will require you to hustle the car back to driver's left and set up along the fog line,

being careful not to drop your left tires before turn-in to Turn 5B.

**Turn 5B and Turn 6:** Turn 5B and Turn 6 lead toward the ascent back up to the elevation of the front straight and therefore getting a good line is important. The turn-in for Turn 5B is from driver's left, right at the fog line. It is important that as you look forward to the apex, you don't drop your left tires as the edge of the track kinks inward before your turn-in point. Make your turn toward the apex and begin to accelerate. The apex is at the outside corner of the FIA curbing on driver's right. You want to set up so that as you pass the apex, you are driving nearly parallel to the fog line on driver's right. As you approach the turn-in for Turn 6, you will need to transition quickly from right to left. The Apex for Turn 6, just past the corner of the FIA curbing on drivers left. Try to be traveling nearly parallel to the last portion of the curbing as you head up the hill toward the track-out on drivers right. Use all the asphalt to get the best acceleration up the hill.



Turn 7, Turn 8 & Turn 9



**Turn 7:** As you head up the hill, keep your right tires near the fog line. As the track turns to the right you will see the turn station directly ahead on track left. Look for the turn-in cone and follow the fog line to the right before turn-in to the apex cone for Turn 7. The apex area of Turn 7 is just beyond the crest of the hill and to make things more interesting, it is physically in a dip in the asphalt. To keep the car settled, stay on the power as you pass the apex and stay out from the apex cone by as much as a car width. Like Turn 1, if you feel uncomfortable, decrease your speed, before turning in toward the apex and then be even on the throttle or accelerating through the apex. At the exit of Turn 7, let the car settle in the right-most painted lane.

**Turn 8 & Turn 9:** As soon as you are parallel with the lane markers between Turn 7 and Turn 8, begin braking, keeping to driver's right. Turn-in is as wide as possible and at a small notch in the asphalt.

The apex is quite late as you want to set up well for Turn 9. The asphalt can have less traction just outside the apex so it is prudent to be precise here. As you track-out about half the width of the track, keep some left steering input to bring the car immediately back to driver's left to set up for Turn 9. You want to be parallel to the track edge, or fog line as you approach turn-in for Turn 9. As you turn in to the apex, if you stay left of the "7" you will be on the proper line. The apex for Turn 9 is at a break in the concrete barriers as they angle onto the front straight. Just past the apex the asphalt undulates a little so keep on the throttle to settle the back end.



**Front Straight:** The front straight has a kink from right to left as it transitions onto the drag strip. The kink is gentle enough to not be considered a turn. Make your line the shortest distance as you go through the kink then transition to driver's left so that you are parallel to the wall when you get to turn-in for Turn 1. Stay a few feet away from the wall as you approach turn-in for 1 as a measure of safety and to stay out of debris that tends to collect there.

CREDITS: Text, photography and graphics developed by Walt Conley, BMW CCA Puget Sound Region Chief Driving Instructor. Additional comments have been provided by Pat Hillyer, PCA Pacific NW Region Chief Driving Instructor.